



डेडीकेटेड फ्रेट कोरीडोर कॉर्पोरेशन

Dedicated Freight Corridor Corporation of India Ltd.  
( A Government of India Under taking )

**Welcomes**  
**All Prospective Applicants**  
**of**  
**Western DFC Phase-I**  
**(SPECIAL STEEL BRIDGES)**  
**&**  
**All Other Dignitaries**



## Dedicated Freight Corridor Western Corridor

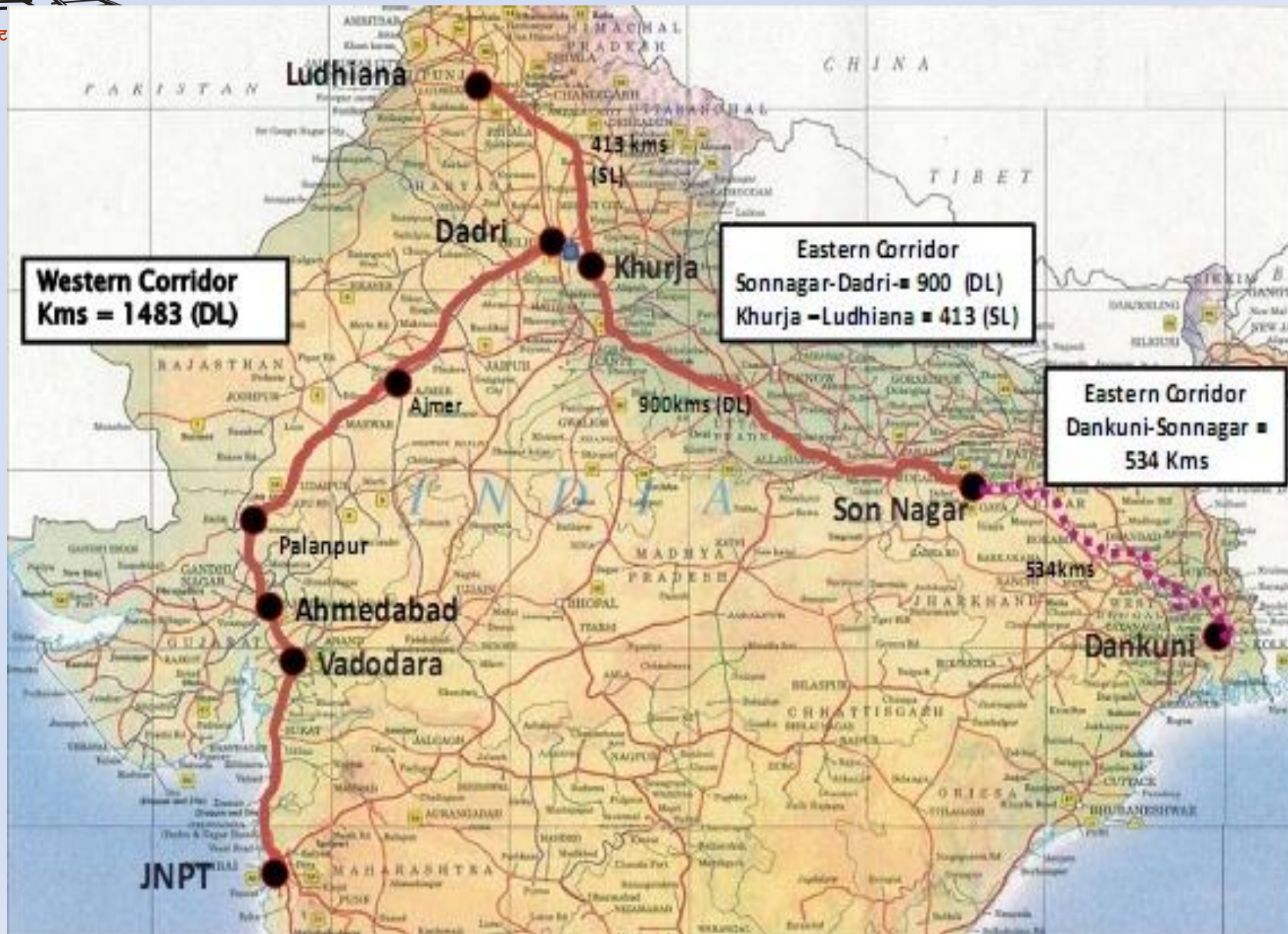
### INVITATION FOR PRE-QUALIFICATION

- **APPLICATIONS FOR PRE-QUALIFICATION FOR PACKAGE 1 AND 2 HAVE BEEN INVITED BY DFCCIL ON DECEMBER 12, 2011.**
- **SUBMISSION OF APPLICATIONS BY APPLICANTS TO DFCCIL : - 27-02-2012**



डेडीकेटेड फ्रेट

## Dedicated Freight Corridor Routes





# Dedicated Freight Corridor - Western Corridor

डेडीकेटेड फ्रेट कोरीडोर कॉर्पोरेशन





# Dedicated Freight Corridor Corporation of India Limited (A Government of India Enterprise)

**GOI HAVE DECIDED TO CONSTRUCT DEDICATED FREIGHT CORRIDORS ALONG EASTERN & WESTERN ROUTES TO MEET:**

- THE SURGING NEEDS OF HEAVY MOVEMENT OF COAL FOR POWER PROJECTS;
- CATER TO BOOMING INFRASTRUCTURE CONSTRUCTION REQUIREMENT; AND
- GROWING INTERNATIONAL TRADE.

*“DEDICATED FREIGHT CORRIDOR CORPORATION OF INDIA LIMITED” (DFCCIL) WAS CREATED ON 30TH OCTOBER 2006 TO UNDERTAKE PLANNING, DEVELOPMENT, MOBILIZATION OF FINANCIAL RESOURCES AND CONSTRUCTION, OPERATION AND MAINTENANCE OF THE DEDICATED FREIGHT CORRIDORS. Two Corridors are planned at present for construction*

- *THE WESTERN CORRIDOR(WDFC) between JAWAHARLAL NEHRU PORT, MUMBAI & DADRI NEAR DELHI.*
- THE EASTERN CORRIDOR BETWEEN LUDHIANA TO SONNAGAR/ DANKUNI
- *BOTH CORRIDOR ALSO INTERCONNECTED BY KHURJA-DADRI LINK*



# Dedicated Freight Corridor Western Corridor

## WESTERN CORRIDOR PROJECT IS PROPOSED TO BE IMPLEMENTED IN TWO PHASES

**PHASE I: Rewari-Makarpura(vadodara) 922km.**

### **PHASE II:**

**Rewari-Dadri 127km**

**Makarpura(Vadodara)- JNPT(Mumbai) 425km**

Procurement : Goods and Service

Under Procurement for implementation of Phase 1 following contracts are under progress-

- i) Contracts for Engineering Consultancy Services for Construction of Western Dedicated Freight Corridor between Rewari – Vadodara & Vadodara-JNPT and Dadri-Rewari
- ii) **Contract Strategy - Design Built Lump sum Contract**
- iii) **Fund ODA STEP Loan from JICA**
- iv) **In order to meet overall 30% STEP obligation under the Loan agreement, utilisation of Japanese Head Hardened Rails for the entire stretch of main line and points and Crossings is mandatory.**



**Dedicated Freight Corridor Corporation of India Limited**  
**(A Government of India Enterprise)**

**THE PROJECT CONTRACT PACKAGES FOR PHASE-I**  
**FOLLOWING CONTRACT PACKAGES ARE PLANNED FOR IMPLEMENTING**  
**PHASE-I**

<b><u>Package No.</u></b>	<b><u>Contract Package</u></b>
<b><u>1</u></b>	<b><u>Civil, Building and Track Works (Rewari – Ajmer)</u></b>
<b><u>2</u></b>	<b><u>Civil, Building and Track Works (Ajmer – Ikbalgarh)</u></b>
<b><u>3</u></b>	<b><u>Civil, Building and Track Works (Ikbalgarh – Vadodra)</u></b>
<b><u>3A</u></b>	<b><u>Construction of special Steel Bridges between Palanpur-Makarpura</u></b>
<b><u>4</u></b>	<b><u>Electrical &amp; Mechanical (E&amp;M) Works (Rewari – Vadodra)</u></b>
<b><u>5</u></b>	<b><u>Signal and Telecommunication (S&amp;T) Works (Rewari – Vadodra)</u></b>
<b><u>6</u></b>	<b><u>Plant and Equipment (P&amp;E) for Operation and Maintenance</u></b>
<b><u>7</u></b>	<b><u>Procurement-cum-Maintenance of Electric Locomotives and construction of a new Maintenance Depot and facilities for Electric Locomotives</u></b>



# Dedicated Freight Corridor Western Corridor

## PROJECT FEATURES WESTERN CORRIDOR – PHASE I

No.	Description	Features
0	<b>Alignment</b>	
		Vadodara-Ajmer-Rewari
1	<b>Route length</b>	
	<b>Total Length</b>	922 km
2	<b>Gradient</b>	
	<b>Ruling Gradient</b>	1 in 200 (5/1000) -Compensated for Curves
	<b>Steepest Gradient in Yards [Compensated for Curves]</b>	1 in 1200 (0.83/1000) 1 in 400 (2.5/1000) in exceptional case
3	<b>Standards of Construction</b>	
	<b>Gauge</b>	1,676 mm
	<b>Rails on Main line</b>	Head Hardened 60 kg/m Rails
	<b>Sleepers for Ballasted Track</b>	PSC 1660 nos./km
	<b>Points &amp; crossings</b>	Head Hardened 60 kg/m Rails, 1 in 12 with Thick-Web Curved Switches and CMS crossings on PSC fan shaped sleepers Minor Loop Lines and Non-running lines, 1 in 8 ½ turnout
	<b>Ballast 65 mm Size</b>	350 mm cushion
	<b>Maximum Train Speed</b>	100 km/h
	<b>Type of Traffic and Axle Load</b>	Double Stack Container(DSC) movement with 25 tonne axle load(At Present)





## Dedicated Freight Corridor Western Corridor

### PROJECT FEATURES WESTERN CORRIDOR – PHASE I - Continued

No.	Description	Features
4	<b>Formation</b>	
	<b>Bank Width for Double Line</b>	<b>13.5 m</b>
	<b>Slope of Embankment</b>	<b>2H: 1V</b>
	<b>Cutting width for Double Line</b>	<b>15.9 m</b>
	<b>Slope of Cutting</b>	<b>1:1</b>
	<b>Blanketing</b>	<b>0.60 m Depth</b>
	<b>Sub-Grade (Maximum)</b>	<b>1.0 m Depth</b>
5	<b>Curves</b>	
	<b>Maximum Degree of Curvature</b>	<b>2.5 degree curve (700 m radius)</b>
	<b>Curve Compensation</b>	<b>At the rate of 0.04 % per degree of curvature</b>
6	<b>Moving dimensions</b>	
	<b>Vertical MMD</b>	<b>7.1 m for Double Stack Container(DSC)</b>
7	<b>Vertical SOD</b>	
		<b>8.43/8.05 m for DSC</b>
8	<b>Track centers</b>	
	<b>Between Two Tracks of DFC</b>	<b>6.0 m</b>
	<b>Minimum Distance Between Existing Track and DFC</b>	<b>7.0 m (Normal) 6.0 m (in exceptional cases)</b>

## Dedicated Freight Corridor Western Corridor

### PROJECT FEATURES WESTERN CORRIDOR – PHASE I - PACKAGE 1 & 2 - Continued

No.	Description	Features
9	<b>Crossing Station</b>	<b>21 Stations</b>
	<b>Junction Stations</b>	<b>8 Stations</b>
10	<b>No of Tunnels</b>	<b>0</b>
11	<b>Signaling System</b>	<b>Automatic Signalling using AF track Circuit with advanced TPWS</b>
	<b>Section length on Double Line</b>	<b>2 km between Stations 1 km nearby station</b>
12	<b>Train Traction System</b>	
	<b>Type of Train</b>	<b>Electric</b>
	<b>Electrification System</b>	<b>25KV AC</b>
	<b>Type of feeding System</b>	<b>AT feeding System (25KVX2)</b>
13	<b>Train operation System</b>	
	<b>Operation type</b>	<b>One manned operation without Brake Van</b>
	<b>Max Speed</b>	<b>100kmph</b>
	<b>Traffic Capacity</b> <b>Double line</b> <b>Single line</b>	<b>140 nos per day direction</b> <b>25 nos per day direction</b>



## Dedicated Freight Corridor Western Corridor

### SPECIAL STEEL BRIDGES CONTRACT PACKAGE (3A)

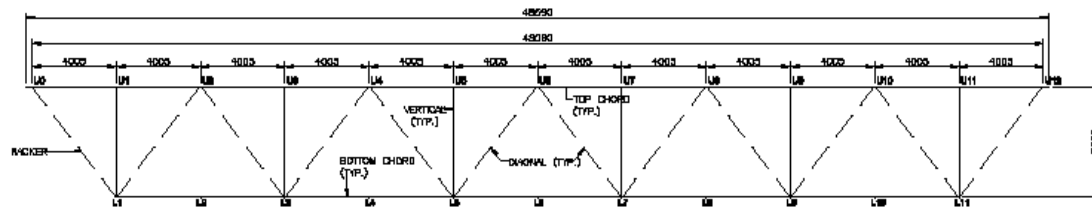
DESIGN AND CONSTRUCTION OF SPECIAL STEEL BRIDGES ACROSS RIVERS MAHI AND SABARMATI INVOLVING BRIDGE STRUCTURE, APPROACHES (OF 100 LENGTH FROM ABUTMENTS ON BOTH SIDES) IN FORMATION IN EMBANKMENTS / CUTTING, GUIDE BUNDS AND PROTECTION WORKS INCLUDING TESTING AND COMMISSIONING ON DESIGN BUILD LUMP SUM PRICE BASIS FOR IQBALGARH-VADODARA SECTION OF WESTERN DEDICATED FREIGHT CORRIDOR

## Dedicated Freight Corridor Western Corridor

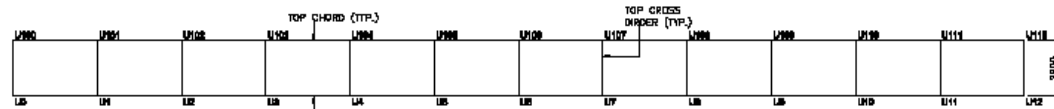
### MAJOR STRUCTURAL FEATURES OF THE TWO BRIDGES

<b>Structural Elements</b>	<b>Bridge across River Mahi (near Vadodara)</b>	<b>Bridge across River Sabarmati (near Ahemdabad)</b>
<b>Bridge length</b>	<b>585 m (12X48.75m)</b>	<b>536.25m (11X48.75m)</b>
<b>Location</b>	<b>Chainage 161/362 km (Sector-5)</b>	<b>Chainage-61/894 (Sector-7)</b>
<b>Type of Bridge</b>	<b>Steel welded Under slung type(with Ballasted deck)</b>	<b>Steel welded Under slung type(with Ballasted deck)</b>
<b>Type of Steel to be used</b>	<b>High performance steel /Higher Strength Steel of grade 57(WB) conforming to IS:8500 Grade B</b>	<b>High performance steel /Higher Strength Steel of grade 57(WB) conforming to IS:8500 Grade B</b>
<b>Type of Foundation</b>	<b>Well Foundation</b>	<b>Well Foundation</b>
<b>Type of River</b>	<b>Perennial</b>	<b>Perennial</b>

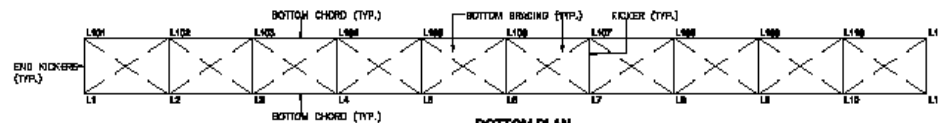




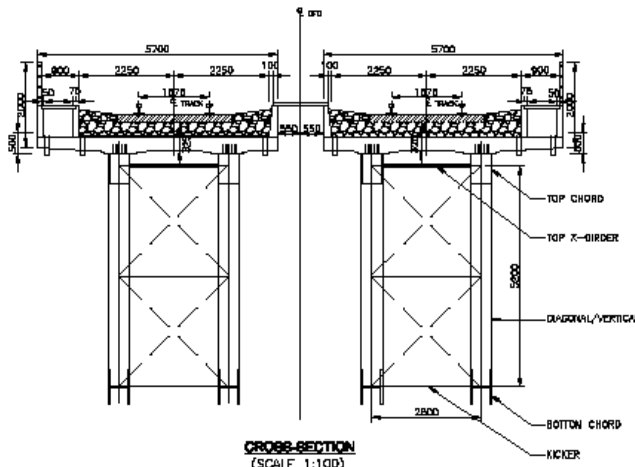
**ELEVATION**  
SCALE: 1:1000



**TOP PLAN**  
SCALE: 1:1000



**BOTTOM PLAN**  
SCALE: 1:1000



**CROSS-SECTION**  
(SCALE: 1:100)

**NOTES :-**

1. DESIGN CRITERIA
  - 1) I.R.S BRIDGE RULES-1984 & DESIGN BASIS REPORT.
  - 2) STEEL BRIDGE CODE-1977 & FATIGUE AS PER DESIGN BASIS REPORT.
  - 3) WELDING BRIDGE CODE-2001
2. THE DESIGN FOR MAIN TRUSS AND FLOOR SYSTEM IS SUITABLE FOR FATIGUE DAMAGE OF TRAIN AS PER DESIGN BASIS REPORT.
3. 80 KG. RAIL USED IN BRIDGE PORTION.
4. WELDABLE MATERIAL (BOTTOM CHORD, TOP CHORD, DIAGONAL MEMBER, VERTICAL, STRINGER S, CROSS GRINDER & GUSSET PLATE) IS AS PER IS: 2062:1982 (GR. C) FULLY KILLED AND FULLY NORMALISED WHEREAS OTHER MATERIAL IS AS PER IS: 2062:1982 (GR. B) SEMI KILLED MINIMUM.
5. AUTOMATIC SUBMERGED ARC WELDING SHALL BE EMPLOYED FOR BUTT WELDS IN FLANGES TO WEB. OTHER WELDS ALSO TO BE MADE BY SUBMERGED ARC WELDING PROCESS TO THE MAX. EXTENT POSSIBLE. THE SPECIFICATION OF WELDING MATERIAL (ELECTRODES) SHALL BE COMPATIBLE WITH PARENT MATERIAL.
6. ALL BUTT WELDS ARE TO BE EXAMINED RADIOGRAPHICALLY OR ANY EQUALLY EFFECTIVE NON DESTRUCTIVE TEST METHOD.
7. ALL WELDS TO BE MADE BY USING APPROVED WELDING PROCEDURES AND BY QUALIFIED WELDERS.
8. TRACK ON BRIDGE SHALL BE PROVIDED WITH GUARD RAIL.
9. UNLESS NOTED OTHERWISE ALL DIMENSIONS ARE IN MILLIMETERS & LEVELS ARE IN METERS.
10. FABRICATION SHALL BE DONE AS PER IRS.- B1 - 2001 & IRS. WELDED BRIDGE CODE UNLESS OTHERWISE NOTED.
11. SIDL INCLUDES THE WEIGHT OF SERVICE PIPE LINE (400 KG./sq) BESIDES RAILS, SLEEPERS AND FIXTURES

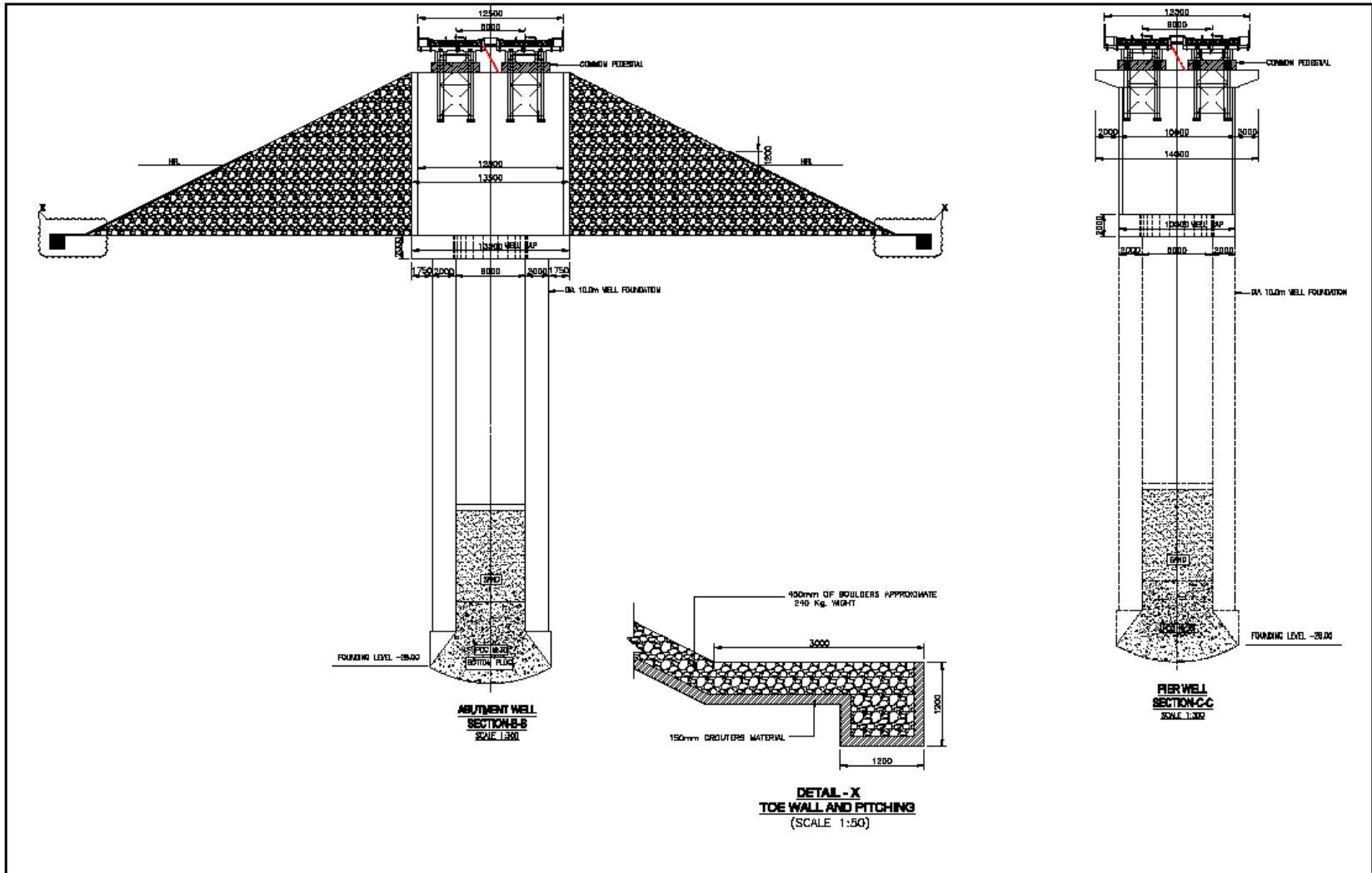
Project Title:  
**Dedicated Freight Corridor Project  
(Western Corridor Phase-1:  
Rewari - Vadodara Section)**



Client:  
  
**Dedicated Freight Corridor Corporation of India**  
5th Floor, Pragati Maidan Metro Station Building,  
New Delhi-110001, INDIA

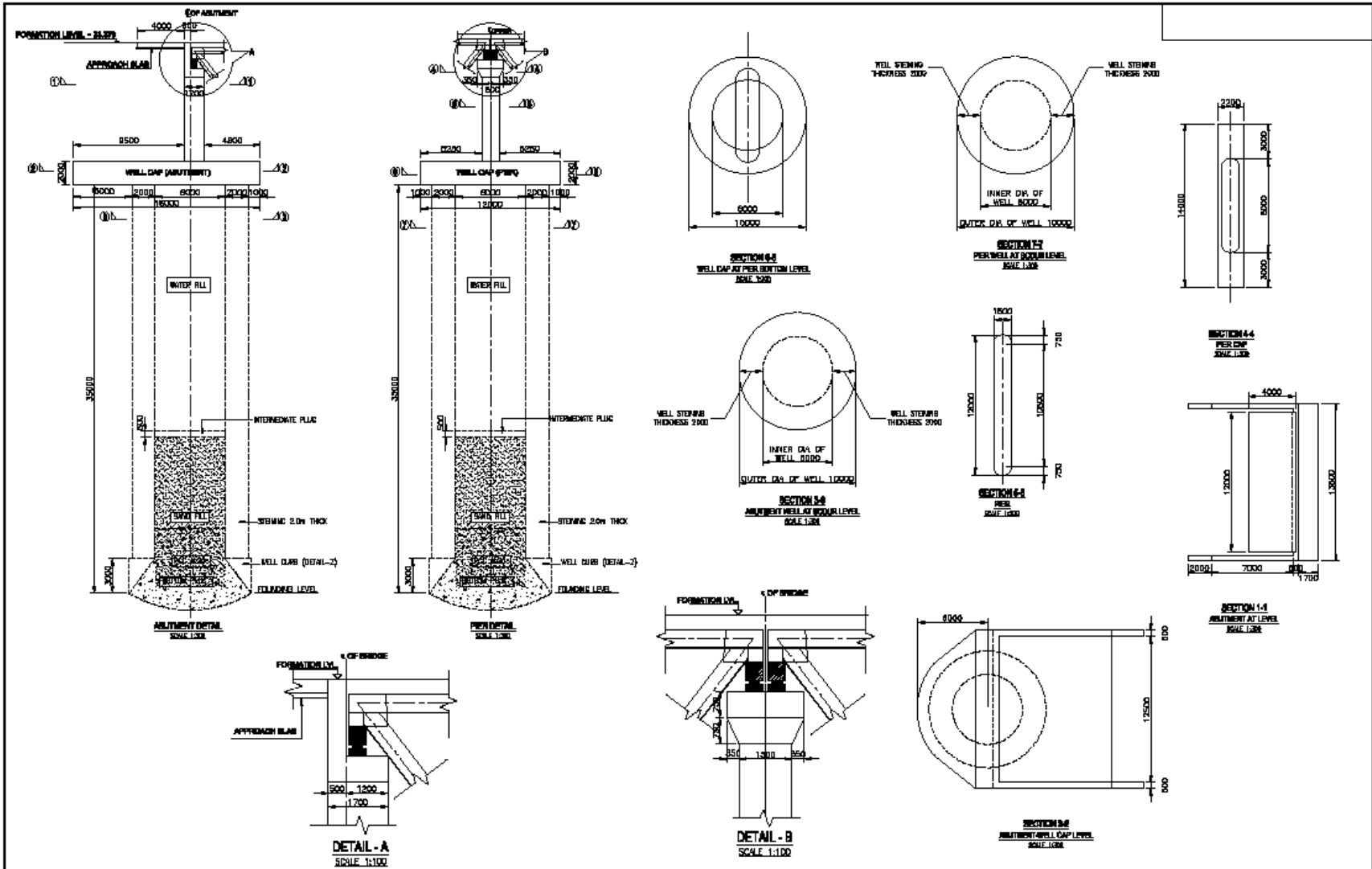
Drawing Title:  
**GENERAL ARRANGEMENT DRAWING  
FOR 12 x 48.75m (PIER CENTER TO PIER  
CENTER) STEEL UNDERSLUNG TRUSS  
WITH COMPOSITE CONCRETE DECK  
(MAHI RIVER)**

Drawing Number:  
**NKC-CTS-IMP-VD-301 (SHEET 5 OF 8)**  
Scale:  
**AS SHOWN**  
Reference:  
**GAD PREPARED BY CPM VADODARA  
(SUB-CONSULTANT:SDCC CONSULTANTS PVT. LTD.)**  
Drawn By: \_\_\_\_\_ Checked By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Consultant:  
 **NK Consortium**  
4th Floor, Pragati Maidan Metro Station Building,  
New Delhi-110001, INDIA



<p>Project Title: <b>Dedicated Freight Corridor Project (Western Corridor Phase-1: Rewari - Vadodara Section)</b></p>	<p>Client:  <b>Dedicated Freight Corridor Corporation of India</b> <small>AN ISO 9001:2015 CERTIFIED ORGANIZATION</small></p> <p>5th Floor, Pragati Maidan Metro Station Building, New Delhi-110001, INDIA</p>	<p>Drawing Title: <b>GENERAL ARRANGEMENT DRAWING FOR 12 x 48.75m (PIER CENTER TO PIER CENTER) STEEL UNDERSLUNG TRUSS WITH COMPOSITE CONCRETE DECK (MAHI RIVER)</b></p>	<p>Drawing Number: <b>NKC-CTS-IMP-VD-301 (SHEET 5 OF 8)</b></p> <p>Scale: <b>AS SHOWN</b></p> <p>Reference: <b>GAD PREPARED BY CPM VADODARA (SUB-CONSULTANT/ROCK CONSULTANTS PVT. LTD.)</b></p> <p>Drawn By: _____ Checked By: _____ Approved By: _____</p>	<p>Consultant:  <b>NK Consortium</b></p> <p>4th Floor, Pragati Maidan Metro Station Building, New Delhi-110001, INDIA</p>
---	---	--	---	--



Project Title:  
**Dedicated Freight Corridor Project  
(Western Corridor Phase-1:  
Rewari - Vadodara Section)**

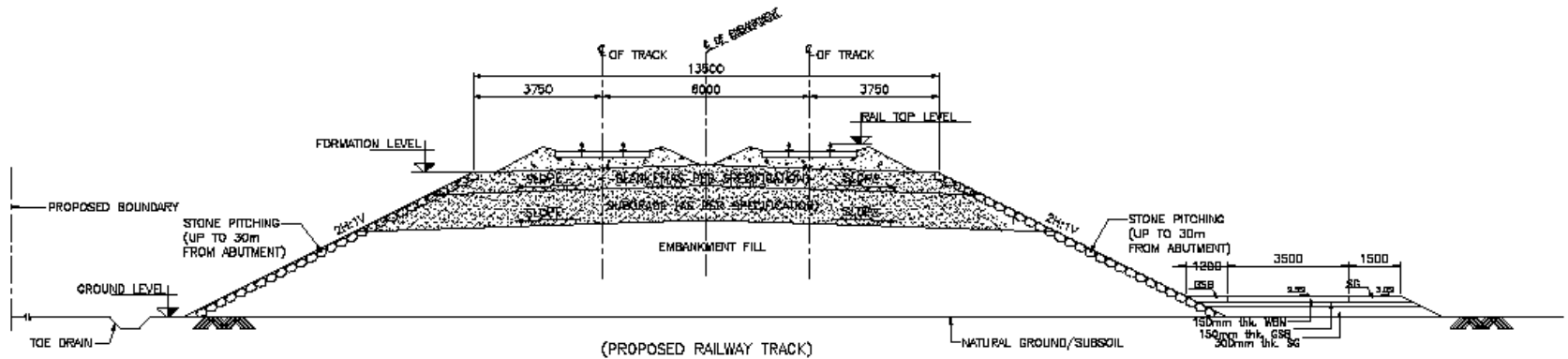
Client:  
 **Dedicated Freight Corridor Corporation of India**  
TO DEVELOP AND OPERATE DEDICATED FREIGHT CORRIDORS  
5th Floor, Pragati Maidan Metro Station Building,  
New Delhi-110001, INDIA

Drawing Title:  
**GENERAL ARRANGEMENT DRAWING  
FOR 12 x 48.75m (PIER CENTER TO PIER  
CENTER) STEEL UNDERSLUNG TRUSS  
WITH COMPOSITE CONCRETE DECK  
(MAHI RIVER)**



Drawing Number:  
**NKC-CTS-IMP-VD-301 (SHEET 7 OF 8)**  
Scale:  
**AS SHOWN**  
Reference:  
GAD PREPARED BY CPM VADODARA  
(SUB-CONSULTANT-ROOIC CONSULTANTS PVT. LTD.)  
Drawn By: \_\_\_\_\_ Checked By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Consultant:  
 **NK Consortium**  
4th Floor, Pragati Maidan Metro Station Building,  
New Delhi-110001, INDIA





TYPICAL X-SECTION OF APPROACH EMBANKMENT WITH SERVICE ROAD  
(SCALE-N.T.S.)

<p>Project Title: <b>Dedicated Freight Corridor Project (Western Corridor Phase-1: Rewari - Vadodara Section)</b></p>	<p>Client:  <b>Dedicated Freight Corridor Corporation of India</b> 5th Floor, Pragati Maidan Metro Station Building, New Delhi-110001, INDIA</p>	<p>Drawing Title: <b>GENERAL ARRANGEMENT DRAWING FOR 12 x 48.75m (PIER CENTER TO PIER CENTER) STEEL UNDERSLUNG TRUSS WITH COMPOSITE CONCRETE DECK (MAHI RIVER)</b></p>	<p>Drawing Number: <b>NKC-CTS-IMP-VD-301 (SHEET 8 OF 8)</b></p> <p>Scale: <b>AS SHOWN</b></p> <p>Reference: <b>GAD PREPARED BY OPM VADODARA (SUB-CONSULTANT/RODIC CONSULTANTS PVT. LTD.)</b></p> <p>Drawn By: _____ Checked By: _____ Approved By: _____</p>	<p>Consultant:  <b>NK Consortium</b> 4th Floor, Pragati Maidan Metro Station Building, New Delhi-110001, INDIA</p>
---	---	--	--	---



## Dedicated Freight Corridor Western Corridor

### BRIEF SCOPE OF WORK

- The proposed work of design and construction of two special steel bridges across Mahi and Sabarmati are located near Valsad (at 161/362 km) and Ahemdabad Station (61/894 km) between Makarpura and Wamaj in Iqbalgarh –Vadodara section of phase-1 of WDFC
  
- THE TYPE OF BRIDGE INVOLVED ARE STEEL WELDED UNDER SLUNG TYPE (WITH BALLASTED DECK) TO BE FABRICATED WITH HIGH TENSILE STEEL GRADE 57 (WB) CONFORMING TO IS:8500. THE FOUNDATION ARE WELL TYPE/PILE. THE WORK WOULD INVOLVE FABRICATION, ERECTION OF OPEN WEB STRUCTURAL STEEL SUPERSTRUCTURE, APPROACHES OF 100M EITHER SIDE FROM ABUTMENTS, GUIDE BUNDS, PROTECTION WORKS AND ALL RELATED WORKS INCLUDING TESTING AND COMMISSIONING INCLUDING AND WITHOUT LIMITATION , THE DESIGN, CONSTRUCTION AND REMOVAL OF TEMPORARY WORKS.
  
- IN ADDITION, CONTRACTOR SHALL UNDERTAKE THE RECTIFICATION OF DEFECTS APPEARING IN THE PERMANENT WORKS IN THE MANNER AND TO THE STANDARDS WITH IN THE TIME STIPULATED BY THE CONTRACT
  
- the Design of all the works will be operationally compatible with the other Indian Railways (IR) sections connecting to the DFC. The track works and system design and its installation shall be carried out through other Contracts.



## Dedicated Freight Corridor Western Corridor

### PERMANENT WORKS

The permanent works shall comprise of but not limited to the design, construction, fabrication / manufacture, supply, installation, testing and commissioning of:

- Survey and investigation including geotechnical investigation including but not limited to setting out, benchmarking etc.
- Bridge substructure including well foundation / pile foundation, well caps / pile caps, piers & pier caps, bearings, approaches of 100m length from abutments on both sides in formation in embankment / cutting, approach slabs, bearings, drainage, guide bund, protection works and other related works etc.;
- Bridge superstructure consisting of fabrication and erection of open web structural steel including trolley refuge
- All the temporary works as required for construction & sinking of well foundations / pile foundations and launching of open web structural steel superstructure across the perennial river
- Any other activity connected with the construction and commissioning of the bridges.

## **Section V. List of Eligible Countries of Japanese ODA Loans**

- Eligibility for the Provision of Goods, Works and Services in
- JICA-Financed Procurement
- (1) The Eligible Nationality of the Supplier(s)/ Applicant and all partners constituting the Applicant shall be the following:
  - (a) Japan in the case of the prime contractor; and
  - (b) All countries and areas in the case of the sub-contractor(s).
- (2) With regard to (1) above, in case where the prime contractor is a joint venture, such joint venture will be eligible provided that the nationality of the lead partner is Japan, that the nationality of the other partners is Japan and/or India and that the total share of work of Japanese partners in the joint venture is more than fifty percent (50%) of the contract amount.
- (3) With regard to (1) and (2) mentioned above,
  - (a) For goods and services, (i) the prime contractor or, in the case of a joint venture, the lead partner and other partners regarded as the Japanese partners shall be nationals of Japan or juridical persons incorporated and registered in Japan, and which have their appropriate facilities for producing or providing the goods and services in Japan and actually conduct their business there; and (ii) in the case of a joint venture, the partners except Japanese partners shall be nationals of India or juridical persons incorporated and registered in Japan or India, and which have their appropriate facilities for producing or providing the goods and services in Japan or India and actually conduct their business there.

# Conflict of interest(COI)

## COI

ITA-4.3- An Applicant shall not have a conflict of interest. All Applicants found to have a conflict of interest shall be disqualified from participating in this prequalification process. An Applicant may be considered to have a conflict of interest with one or more parties in this prequalification process if:

- (a) an Applicant has been engaged by the Employer to provide consulting services for the preparation related to procurement for or implementation of the project;
- (b) an Applicant is any of its associates/affiliates (inclusive of parent firms) mentioned in subparagraph (a) above; or
- (c) an Applicant lends, or temporarily seconds its personnel to firms or organizations which are engaged in consulting services for the preparation related to procurement for or implementation of the project, if the personnel would be involved in any capacity on the same project.

## Requirement

ITA-4.3(PDS)- As part of the Application for Pre-qualification, the Applicant shall submit an undertaking from each of the proposed Partners and Sub-contractors / Sub-consultants, duly signed by their Authorized Signatories, that they do not have any conflict of interest under the provisions of Clause ITA 4.3 (a), (b) & (c).

# Conflict of Interest Contd...

**As per information provided by E&S Consultants, Following firms are currently associated with E&S Consultants**

- 1.M/s Wilbur Smith Associates, India**
- 2. M/s DHI(India) Water & Environment Pvt. Ltd**
- 3.M/s Scott Wilson India Pvt. Ltd**
- 4.CH2M Hill (India) Pvt. Ltd**
- 5.Consulting Engineering Services(India) Pvt, Ltd(CES)**

Plus

29.1(c ) (PDS) – the Employer shall not consider prequalification of any applicant by omitting any ineligible partner / Sub-contractor/ Sub-consultant , included in the application for Prequalification.

# Credentials

## ITA 24.1 (PDS)

- The Credentials of Associates, Parent Companies or Sister Companies shall not be considered unless they are part of joint venture / association.
- Both in case of 100% owned SPVs as well as subsidiaries, with respect to (a) Technology (b) Financials and (c) Intellectual Property Rights, the parent company can aggregate these for the purpose of qualification criteria so long as the parent company prepares a consolidated balance sheet that includes the financials of these subsidiaries and SPVs.
- However, this will not be permitted in case of clause no. 4.2(b) of Section III i.e. experience in key activities undertaken by subsidiaries (including 100% SPVs) will not qualify as experience of Parent Company unless the subsidiaries (including 100% SPVs) are also part of the Consortiums /Joint Venture/Association.
- Rules for Rounding off Numerical Values : While evaluating the quantitative credentials of the Applicant, the decimal values shall be rounded off to the next higher digit e.g a figure of say 14.1 shall be rounded off to 15 for evaluation purpose.

# Banning of Business

## ITA 4.7(PDS)

- **The Applicant shall be considered in-eligible if any of its Partners and / or Sub Contractors / Sub Consultants included in the Application for Prequalification has been banned for business with Ministry of Railways along with any of its attached and subordinate offices through an order issued by Ministry of Railways as per list available on Web site ( <http://www.indianrailways.gov.in/railwayboard> ) of Civil Engg. Directorate of Railway Board pertaining to Banning of Business, with the Banning being valid as on the last date of submission the PQ Application.**
- **The Applicant / each partner of the joint venture or association including the proposed sub-contractors, sub-consultants shall submit an undertaking to this effect duly signed by their authorized signatory along with the Application for prequalification.**



# General definitions

## ITA 4.1 (PDS)

- For the purpose of this pre-qualification document the following definition shall apply:
- **'Applicant'** : It is the collective single entity consisting of **'Prime Contractor'** with its **'Sub-contractor(s) & Sub-consultants'**
- **'Prime Contractor'** may be single entity or **'a joint venture or partner in JV'** or **'association or its member'** excluding **Sub Contractor' & Sub Consultants** with the formal intent, as evidenced by a letter of intent, to enter into an agreement or under an existing/pre-existing agreement
- **'Partners'** : **Prime Contractor** or in case where the **Prime Contractor** is a joint venture or association , **'Partners'** would include all the constituent members of the joint venture or association.
- **'Authorised Signatory'**: For the purpose of this pre-qualification, the **Authorised Signatory** shall mean the person authorized to sign on behalf of **Partner / Sub-contractor/ Sub consultant** under a **Power of Attorney** and supported by a resolution from the respective **Board of Directors**

# Documentary Evidence in support of experience

## ITA 14.1(PDS)

**The Applicant shall also submit Documentary evidence / Client's Certificates in support of the Applicant's claim of the Experience (General and Specific) in respect of each contract as included in Section IV, Application Forms In case of the works / financial situations involving foreign currencies, the same shall be converted to USD by first converting it to Japanese Yen and then to USD by applying the foreign exchange rate as specified in Section III : Qualification Criteria and Requirements.**

**The experience certificate issued by the Engineer acting on behalf of the Client shall also be considered provided the documentary evidence, establishing the appointment of the said Engineer by the Client. is also submitted along with the experience certificate issued by the said Engineer,**

In case of PPP projects, the Concessionaire can give experience certificate in respect of the EPC/Construction Contractor.

However such certificate will need to be countersigned by :  
The concerned Public Authority who has granted the concession clearly

- indicating the name, designation and official address of the signing authority
- OR
- The Independent Engineer appointed by the concerned Public Authority who has granted the concession clearly indicating the name, designation and official address of the signing authority. The document(s) establishing the appointment of the said Independent Engineer shall also be required.
- 
- OR
- Alternatively, the Applicant shall submit a notarized set of documents comprising of
- A Self certification of the work undertaken, period of execution, quantities and money value
- Certificate by the Concessionaire indicating work undertaken, period of execution, quantities and money value
- 
- Or
- b-1)Certificate from EPC/Construction Contractor who has subsequently been awarded the Contract by the Concessionaire, provided identity of concessionaire and EPC contractor is adequately established through following documents
- b-1.i) Certificate from the Public Authority indicating the granting of concession i.e. establishing the identity of the concessionaire.
-

- **b-1.ii) Certificate from the Concessionaire to establish the award of Contract to the EPC Contractor.**
- 
- **b-1.iii) Letter/communication from EPC to Concessionaire communicating the engagement of the Applicant as EPC Sub Contractor.**
- **b-1.iv) Letter/communication from Concessionaire to Public Authority communicating the engagement of the Applicant as EPC Sub Contractor.**
- **Work Order / Contract Agreement with the Concessionaire or in case, the Sub clause (b-1) is applicable,**
- 
- **c-1) Work order/Contract agreement with the EPC contractor ( who has been awarded Contract by the Concessionaire) (in case of clause no-(b-1) being applicable)**
- 
- **Final Bill / Last Running Account Bill duly certified by the Chartered Accountant for the payment received.**
- 
- **In such a case, the quantities executed by the Applicant during a calendar year / financial year shall be assessed on pro-rata basis for the period from commencement of the project to the date of the Bill.**
- **Note- In case of any Applicant/Sub Contractor claims execution of certain work in order to fulfill eligibility criteria which has also been claimed by some other Applicant/Sub Contractor, that experience of work in question would not be aggregated while evaluating the eligibility of both the involved Applicants**

## Dedicated Freight Corridor Western Corridor

### PRE-QUALIFYING CRITERIA

**TO PRE-QUALIFY, THE APPLICANTS MUST MEET THE FOLLOWING KEY QUALIFYING CRITERIA EITHER AS SINGLE ENTITY OR AS JOINT VENTURE**

<p><b>FINANCIAL PERFORMANCE (COMMON FOR ALL PACKAGES)</b> SUB CLAUSE 3.1</p>	<p><b>FOR THE LAST FIVE (5) YEARS APPLICANT TO DEMONSTRATE THE CURRENT SOUNDNESS OF THE APPLICANT'S FINANCIAL POSITION AND ITS PROSPECTIVE LONG TERM PROFITABILITY.</b></p> <p><b>APPLICANT'S NET WORTH CALCULATED AS THE DIFFERENCE BETWEEN TOTAL ASSETS AND TOTAL LIABILITIES SHOULD BE POSITIVE FOR TOTAL OF FIVE (5) YEARS.</b></p> <p><b>THE TOTAL LIABILITIES INCLUDE LONG-TERM AND SHORT TERM LIABILITIES.</b></p>
<p><b>AVERAGE ANNUAL CONSTRUCTION TURNOVER (PER PACKAGE)</b> SUB CLAUSE 3.2</p>	<p><b>MINIMUM AVERAGE ANNUAL CONSTRUCTION TURNOVER OF US\$ 30 MILLION, CALCULATED AS TOTAL CERTIFIED PAYMENTS RECEIVED FOR CONTRACTS IN PROGRESS OR COMPLETED, WITHIN THE LAST FIVE (5) YEARS.</b></p> <p><b>THE AVERAGE ANNUAL CONSTRUCTION TURNOVER RELATED TO ALL ACTIVITIES CONSISTING OF BUILDING/ ASSEMBLING /ERECTION OF PHYSICAL ASSETS, TRANSPORTATION INFRASTRUCTURE, MANUFACTURING PLANTS, PIPE LINES, POWER PROJECTS, TRANSMISSION LINES, STEEL STRUCTURE, RAILWAY PROJECT, REFINERIES ETC.</b></p>



## Dedicated Freight Corridor Western Corridor

### PRE-QUALIFYING CRITERIA - Continued

#### **NOTES/CLARIFICATIONS:**

#### **SUB CLAUSE 3.2: AVERAGE ANNUAL CONSTRUCTION TURNOVER (PER PACKAGE)**

#### **IN CASE OF JOINT VENTURE/ASSOCIATION:**

- **ATLEAST ONE PARTNER MUST MEET 40% OF THE REQUIREMENT;**
- **EACH PARTNER MUST MEET 25% OF THE REQUIREMENT;**
- **THE LEAD PARTNER SHOULD HAVE MAXIMUM FINANCIAL STAKE AMONGST THE OTHER JV PARTNER/S OR ASSOCIATION PARTNER/S IN THE JV AGREEMENT FOR THE PROJECT;**
- **THE INDIVIDUALS OR FIRMS IN A JOINT VENTURE OR ASSOCIATION SHALL BE JOINTLY AND SEVERALLY LIABLE; AND**
- **THERE IS NO LIMITATION ON NUMBER OF JV PARTNERS.**

#### **EXCHANGE RATE FORM FIN 3.2:**

**THE FOREIGN EXCHANGE RATE SHALL BE TTS RATE OF BANK OF TOKYO MITSUBISHI –UFJ(BTMU) AS ANNOUNCED ON 28 DAYS PRIOR TO FINAL SUBMISSION DATE OF APPLICATION FOR PRE-QUALIFICATION.**



# Dedicated Freight Corridor Western Corridor

## PRE-QUALIFYING CRITERIA - Continued

<p><b>GENERAL CONSTRUCTION EXPERIENCE</b> SUB-CLAUSE 4.1</p>	<p>EXPERIENCE UNDER CONSTRUCTION CONTRACTS IN THE ROLE OF CONTRACTOR, MANAGEMENT CONTRACTOR, OR SUBCONTRACTOR, <sup>1</sup> FOR AT LEAST THE LAST <u>FIVE (5) YEARS</u> PRIOR TO THE APPLICATION SUBMISSION DEADLINE, AND WITH <u>ACTIVITY IN AT LEAST NINE (9) MONTHS IN EACH YEAR.</u></p>
<p><b>SPECIFIC CONSTRUCTION EXPERIENCE</b> SUB-CLAUSE 4.2 (a)</p>	<p>EXPERIENCE EXCLUSIVELY IN THE ROLE OF PRIME CONTRACTOR OR PARTNER, IN EXISTING OR PAST JV “IN AT LEAST (A) A SINGLE CONTRACT OF 30 MILLION USD (B) TWO (2) CONTRACTS OF 20 MILLION USD EACH WITHIN THE LAST TEN (10) YEARS, (PRIOR TO THE APPLICATION SUBMISSION DEADLINE) THAT HAVE BEEN SUBSTANTIALLY COMPLETED OR HAVE RECEIVED TAKING-OVER CERTIFICATE FOR WHOLE OF THE WORKS AS THE RESULT OF SUCCESSFULLY AND SUBSTANTIALLY COMPLETED AND THAT ARE SIMILAR TO THE PROPOSED WORKS. THE PROJECTS SHALL BE CONSIDERED TO HAVE BEEN SUBSTANTIALLY COMPLETED IF THE APPLICANT HAS RECEIVED PAYMENT AGAINST THE WORKS DONE TO THE EXTENT OF 90% OR MORE OF THE CONTRACT VALUE</p> <p>THE SIMILARITY SHALL BE BASED ON THE PHYSICAL SIZE, COMPLEXITY, METHODS/TECHNOLOGY OR OTHER CHARACTERISTICS AS DESCRIBED IN SECTION VI, SCOPE OF WORKS. THE WORKS PERTAINING TO RAILWAYS, HIGHWAYS, SEA/AIR PORTS, DAMS/ HYDROELECTRIC POWER PROJECTS (HPP), AND IRRIGATION PROJECTS SHALL ONLY BE CONSIDERED. IN ADDITION TO THE ABOVE, THE BUILDING WORKS EXECUTED IN THERMAL POWER PROJECTS, HEAVY INDUSTRIES AND REFINERIES SHALL ALSO BE CONSIDERED.</p>



## Dedicated Freight Corridor Western Corridor

### PRE-QUALIFYING CRITERIA - Continued

**SPECIFIC  
CONSTRUCTI  
ON  
EXPERIENCE  
SUB-CLAUSE  
4.2 (b)**

**FOR THE ABOVE OR OTHER CONTRACTS EXECUTED DURING THE PERIOD STIPULATED IN 4.2 (a) ABOVE, A CONSTRUCTION EXPERIENCE, AS PRIME CONTRACTOR, MANAGEMENT CONTRACTOR, OR SUBCONTRACTOR, IN THE FOLLOWING KEY ACTIVITIES:**

**(1) MINIMUM 250M OF WELL SINKING OF DIA NOT LESS THAN 6M FOR MAJOR BRIDGES/IMPORTANT BRIDGES ACROSS PERENNIAL RIVERS/LAKES/SEA/SEA CREEKS IN ANY ONE YEAR (CALENDAR YEAR OR FINANCIAL YEAR)**

**OR**

**MINIMUM 9500M OF PILING OF DIA NOT LESS THAN 1.0M FOR MAJOR BRIDGES/IMPORTANT BRIDGES ACROSS PERENNIAL RIVERS/LAKES/SEA/SEA CREEKS IN ANY ONE YEAR (CALENDAR YEAR OR FINANCIAL YEAR)**

**(1) MINIMUM LENGTH OF 300 M OF FABRICATION AND ERECTION OF OPEN WEB STRUCTURAL STEEL SUPERSTRUCTURE IN DIFFERENT SPANS WITH INDIVIDUAL SPAN LENGTHS NOT LESS THAN 30.5M EACH FOR MAJOR/IMPORTANT BRIDGES ACROSS PERENNIAL RIVERS/LAKES/SEA/SEA CREEKS IN ANY ONE YEAR (CALENDAR YEAR OR FINANCIAL YEAR)**





## Dedicated Freight Corridor Western Corridor

### PRE-QUALIFYING CRITERIA - Continued

**GENERAL  
DESIGN  
EXPERIENCE  
SUB-CLAUSE 4.3  
(a)**

Experience exclusively in the role of prime contractor, subcontractor, design consultant or sub-design consultant, in at least two (2) design contracts of not less than US\$ 300,000 each for design of Major Bridges / Important Bridges across perennial rivers / lakes / sea / sea creeks within the last ten (10) years, that have been Substantially Completed or have received Taking-over Certificate for whole of the Works as the result of successfully and substantially completed and that are similar to the proposed Works. The project shall be considered to have been Substantially Completed if the Applicant has received payment against the work done to the extent of 90% or more of the contract value

The works pertaining to Railways, Highways, Sea Ports Projects shall only be considered.

# Design criteria contd....

<p><b>SPECIFIC DESIGN EXPERIENCE (PER PACKAGE ) SUB-CLAUSE 4.3 (B)</b></p>	<p><b>FOR THE ABOVE OR OTHER CONTRACTS EXECUTED DURING THE PERIOD STIPULATED IN 4.3 (a) ABOVE, A MINIMUM DESIGN EXPERIENCE IN THE KEY ACTIVITIES:</b></p> <p>Should have designed:</p> <p>Well Foundation of dia not less than 6m</p> <p>OR</p> <p>Pile Foundation of dia not less than 1m for two (2) Major Bridges / Important Bridges across perennial rivers / lakes / sea / sea creeks</p> <p>and</p> <p>Should have designed the open web structural steel superstructure for two (2) Major Bridges / Important Bridges across perennial rivers / lakes / sea / sea creeks of similar nature (physical size, complexity and method forms) as proposed.</p>
--	--

# Check List

- From Page no-39 to 41 of PQ document, you would find a detailed Check List which would have to be mandatorily filled in as per given format. Under Column marked Reference, details of page number of PQ application being submitted would have to be specifically referred

# Letter of Intent

• **Form ELI – 1.1A**

• **Letter of Intent to Enter in to an Agreement**

• *(To be signed by all the Partners of joint venture or association)*

• *(Reference Clause 4.1 (PDS) and 11.1 (d) (PDS)*

• Mr. Dhanesh Gupta , GGM / PWC

• Dedicated Freight Corridor Corporation of India Limited (DFCCIL)

• Room No. 505, 5th Floor, Pragati Maidan Metro Station Building,

• New Delhi – 110001, India

• Subject : Pre-qualification for “Design and Construction of Civil, Building and Track Works for Double Line Railway Involving Formation in Embankments / Cuttings, Bridges, Structures, Buildings, Ballast on Formation, Track Works Including Testing and Commissioning on Design-Build Lump Sum Price Basis for Rewari – Ikbargarh Section of Western Dedicated Freight Corridor” Project – Civil, Building and Track Works Contract Package 1 and Package 2

• Dear Sir,

• This is to inform you that :

• We, the following entities

• ----- (name and address of Lead Partner)

• -----(name and address of Partner)

• -----(name and address of Partner)

• -----

• hereby express our intention that we are forming a joint venture / association under the name of ----- (if any), to participate in the Pre- qualification process of the subject work.

• We nominate ----- as Lead Partner of our joint venture / association.

• We undertake that the Lead Partner of our joint venture/association ----- (Name of the lead partner) shall have the maximum financial stake amongst the other partners of the joint venture / association.

• We undertake that total share of the Japanese partners in our joint venture / association shall be more than 50% of the total contract amount.

• The percentage participation amongst various partners of the joint venture / association for the subject project shall be as under:

• Sl. No.

• Name of the Partner

• Role ( Lead partner/ partner)

• % Participation

• In accordance with Clause ITA 4.1 (a) (PDS) of Section I, we shall be jointly and severally liable to DFCCIL for the execution of the Contract in accordance with the Contract Terms

• In accordance with Clause ITA 4.2 (b) (PDS) of Section I, We shall nominate the following person as our authorized signatory who shall have the authority to conduct the business for and on behalf of our Joint venture / association during the prequalification process and , in the event of joint venture /association is prequalified, during the bidding process, and in the event the joint venture/association is awarded the Contract, during Contract execution

• (Detailed information about authorized signatory)

• Name,

Designation,

Address,

Tel/Fax no

E Mail ID

• In case we are pre-qualified, we shall be entering in to a detailed Joint Venture / Association Agreement at the time of submission of our Bid

• Authorised Signatory

• (Lead Partner)

Authorised Signatory

(Partner)

• Authorised Signatory

• (Partner)

Authorised Signatory

(Partner)

# Encapsulation of earlier Q&A session

S No.	Questions raised	Client response
1	The work is to be executed in India, we are forming a Joint Venture/Consortium with Japanese party as a Lead member of consortium and we are mentioning official address of consortium in India, whether it is acceptable	Yes
2	kindly confirm that the submission certificates duly certified by Client (mentioning work experience) does not required to be endorsed by public notary and/or Indian Embassy/ <b>Hague Convention</b> apostille for the lead Partner.	Public notary and/or Indian Embassy/ <b>Hague Convention</b> apostille certification for the lead Partner is not required.
3	Kindly specify the maximum no of JV partners allowed	No limitation.

# Encapsulation of earlier Q&A session..

S No.	Questions raised	Client response
4	Kindly clarify if the PQ documents can be purchased by any one member on behalf of JV	Yes
5	Whether the entire land required for the development of corridor, including stations and detours is in the possession of the employer (DFCCIL).	DFCCIL has acquired more than 90% of the land required (ROW) for these two packages. It is envisaged that the land required for these two packages will be in possession of DFCCIL before the DB Contractor is in position
6	Whether post bidding the work shall be awarded to a single lowest bidder in both the packages.	Yes, provided they qualify for both the packages

# Encapsulation of earlier Q&A session..

S No.	Questions raised	Client response
7	<p>The Applicant does not have any litigations which may negatively impact to its performance of this project, however, there are several small litigations which does not affect for this project. Even in case it is obvious that all pending litigation (including arbitration) in total represents very small comparing with the Applicant's net worth and do not affect to the execution of the contract(s), is it still required to list up all litigation cases? Please kindly consider waiving such requirement in the above case.</p>	<p>Applicant is required to list all of pending litigations (including arbitration) in the construction contracts.</p>

# Encapsulation of earlier Q&A session..

S No.	Questions raised	Client response
8	In case qualifying works have been executed in joint venture, we understand credit proportionate to % participation in JV or as per actual scope shall be considered. Please confirm	Yes, Please quantify on an actual work execution basis in a JV.
9	Experience related to Key activities, can it be met by adding the experiences of different partners? Experience related to key activities would be two times for both the packages?	Yes,  Yes



# Important Bridges (Phase-2)

S no	Chainage	Span	Bridge length (meter)	Structure type (meter)	Crossing River
01	53/945	6X76.2	457.2	Steel Girder	Ulhas
02	9/642	6X48.15	289.0	Steel through Girder	Damanganga
03	029/917	5X48.768	244.0	Through Girder	Par
04	23/025 (Surat detour)	11X45.7	502.7	Through Girder	Tapi
05	73/658	32X45.1	1443.2	PSC Box Girder	Namada
06	126/865	11X48.15	529.7	-	Yamuna
07	132/083	9X48.15	433.4	Steel through	Hindon



**THANKS**