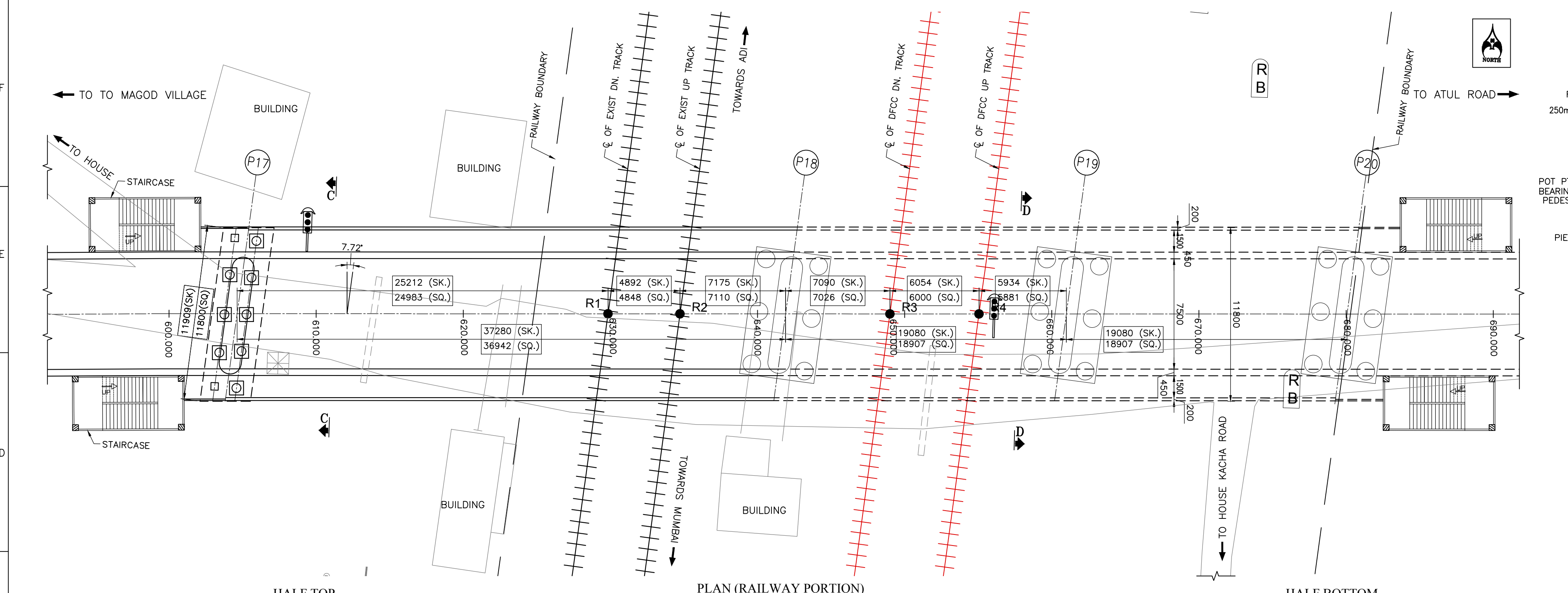
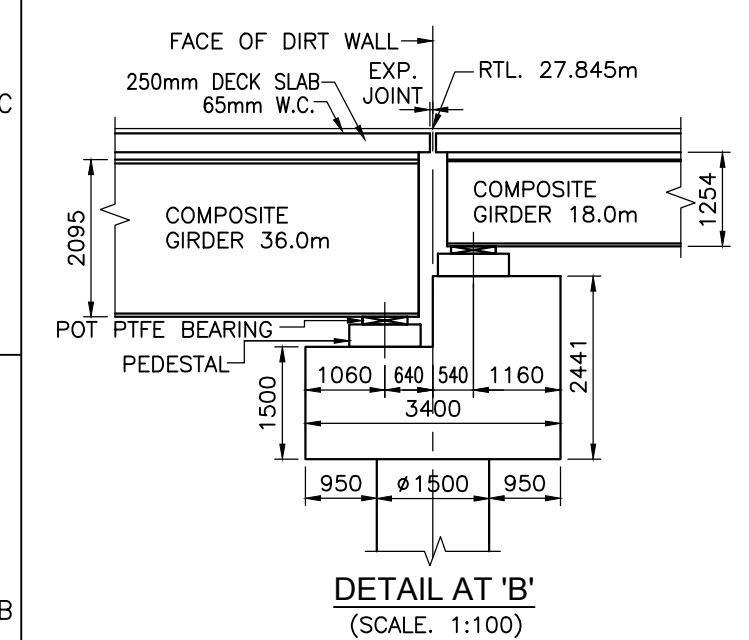


	P17	P18	P19	P20
DATUM LVL. 8.000				
PROP. ROAD TOP LEVEL	27.845	27.845	27.845	27.845
PIER CAP TOP LEVEL	24.937	24.937/25.878	25.519	25.519
GROUND LEVELS	16.761	17.161	16.453	17.166
PILE CAP TOP LEVEL	16.261	16.661	15.953	16.666
PILE CAP BOTTOM LEVEL	14.461	14.861	14.153	14.866
CHAINAGE	604.100	641.380	660.460	679.540

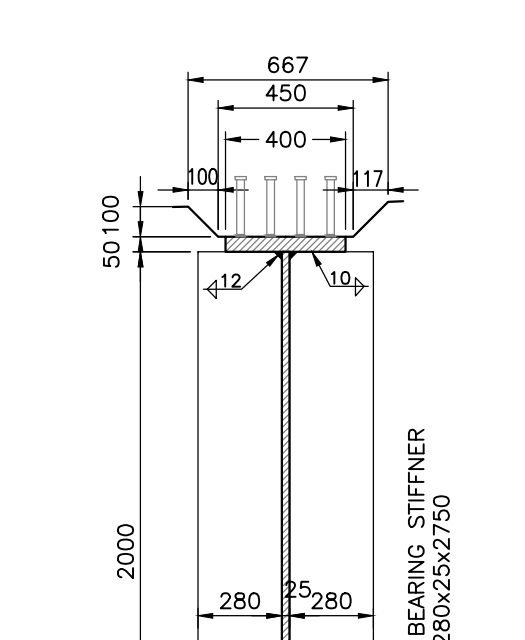
ELEVATION (RAILWAY PORTION)
(SCALE: 1:200)



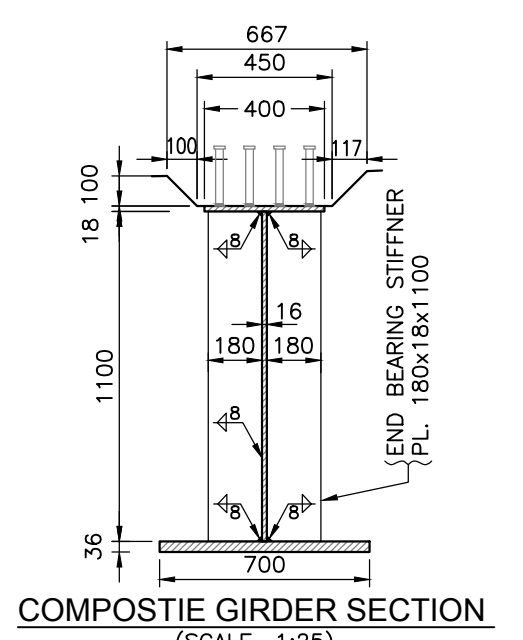
PLAN (RAILWAY PORTION)
(SCALE: 1:300)



DETAIL AT 'A'
(SCALE: 1:100)

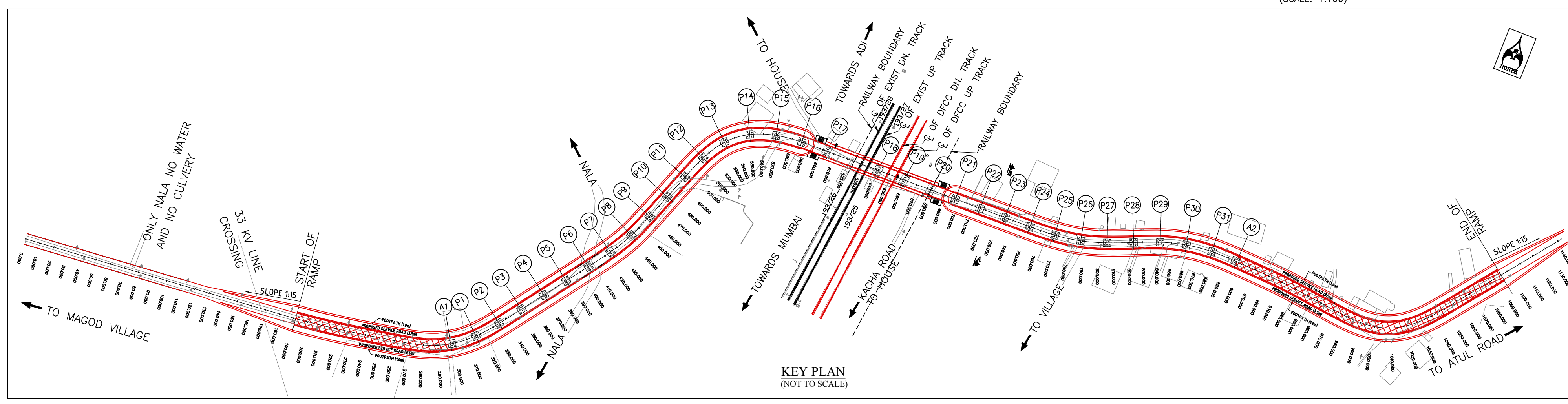


COMPOSITE GIRDER SECTION
(SCALE: 1:25)

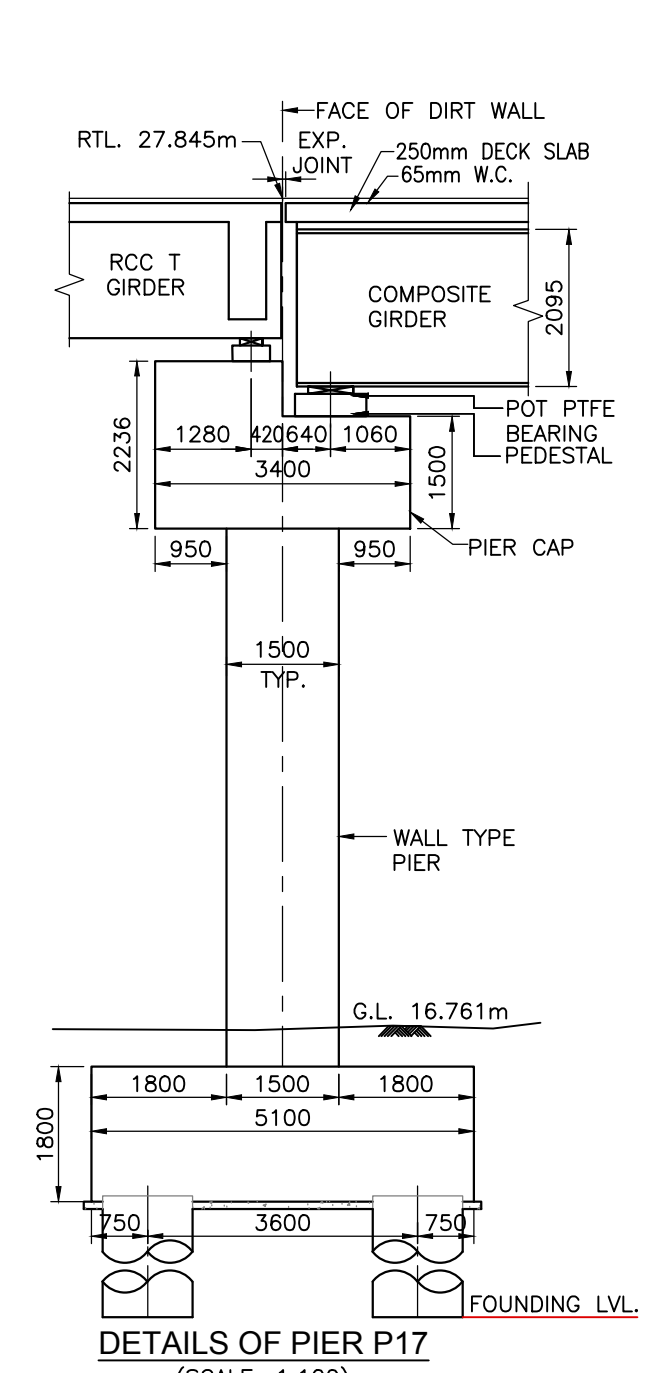


COMPOSITE GIRDER SECTION
(SCALE: 1:25)

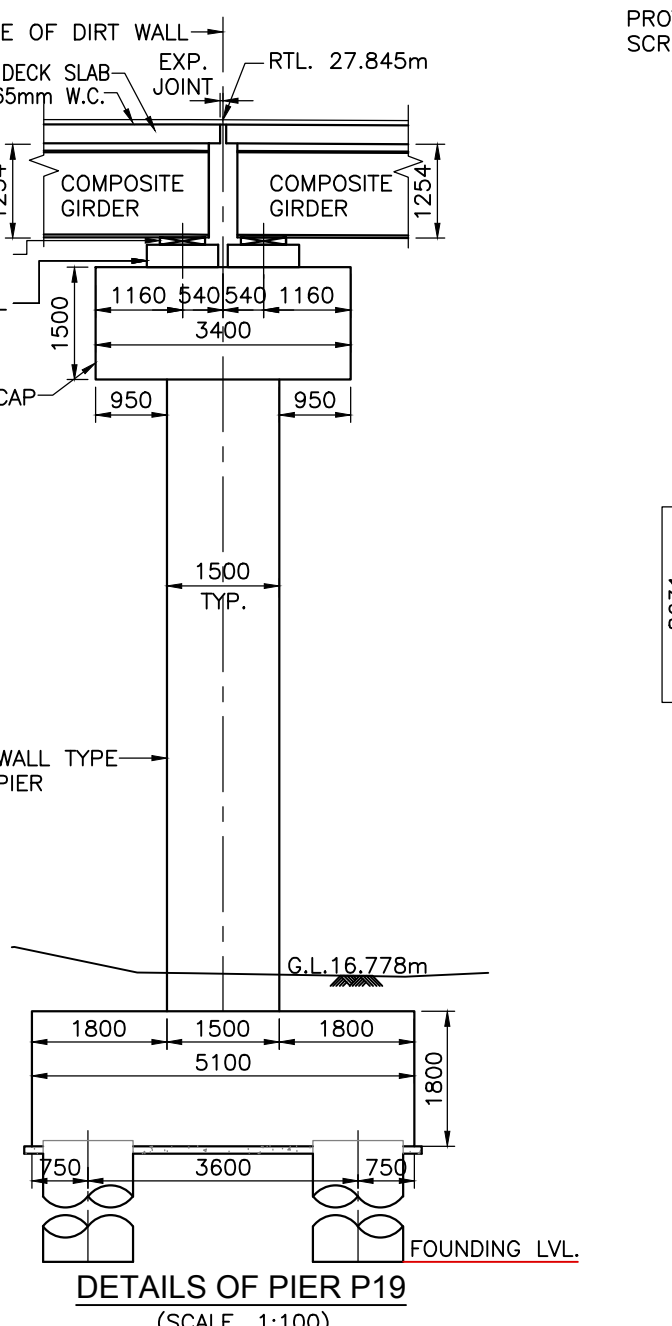
TRACK NO.	RAIL TOP LEVEL	LOCATION
EXIST DN. TRACK	17.505m	R1
EXIST UP. TRACK	17.422m	R2
DFCC DN. TRACK	17.645m	R3
DFCC UP. TRACK	17.645m	R4



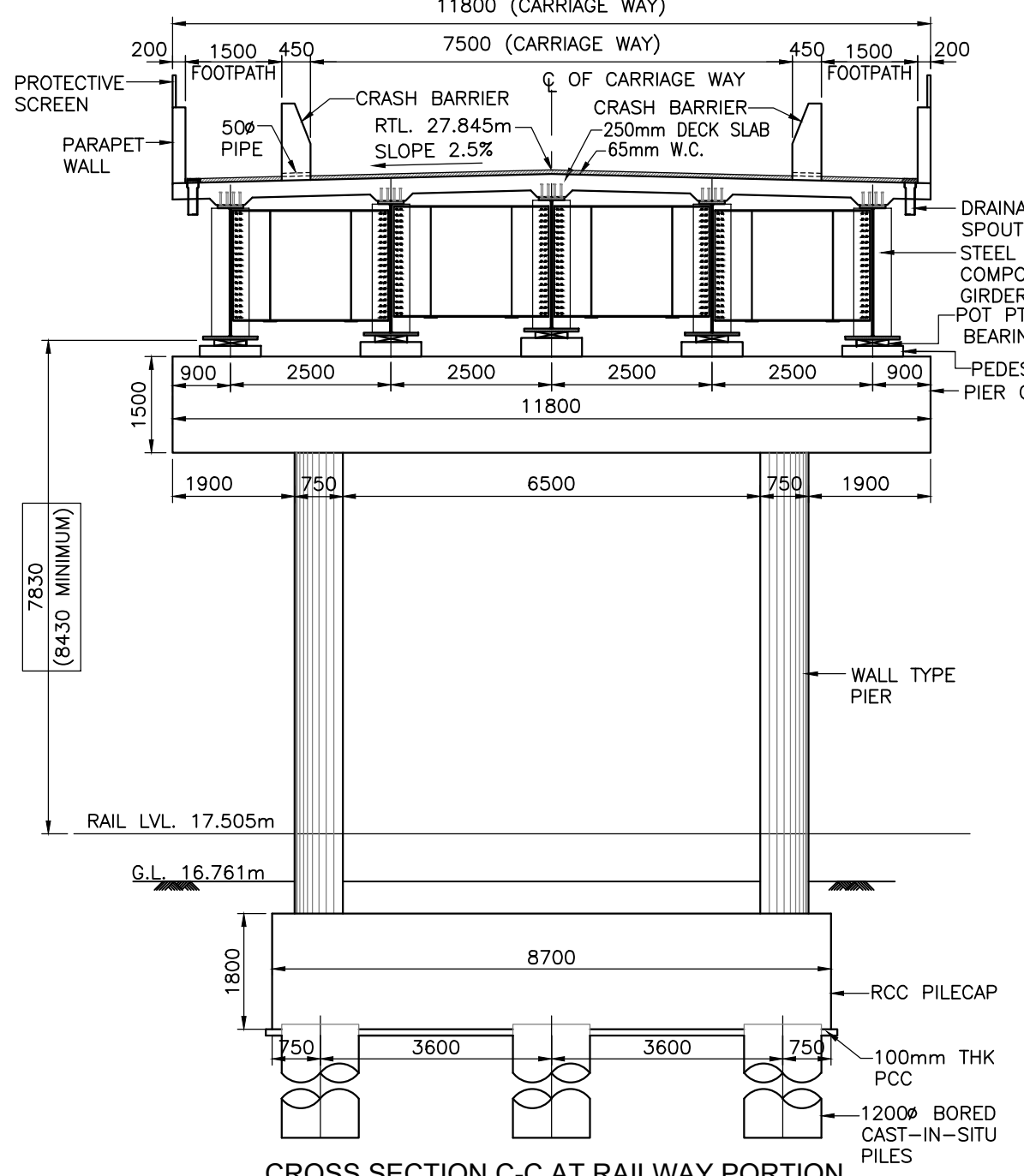
KEY PLAN
(NOT TO SCALE)



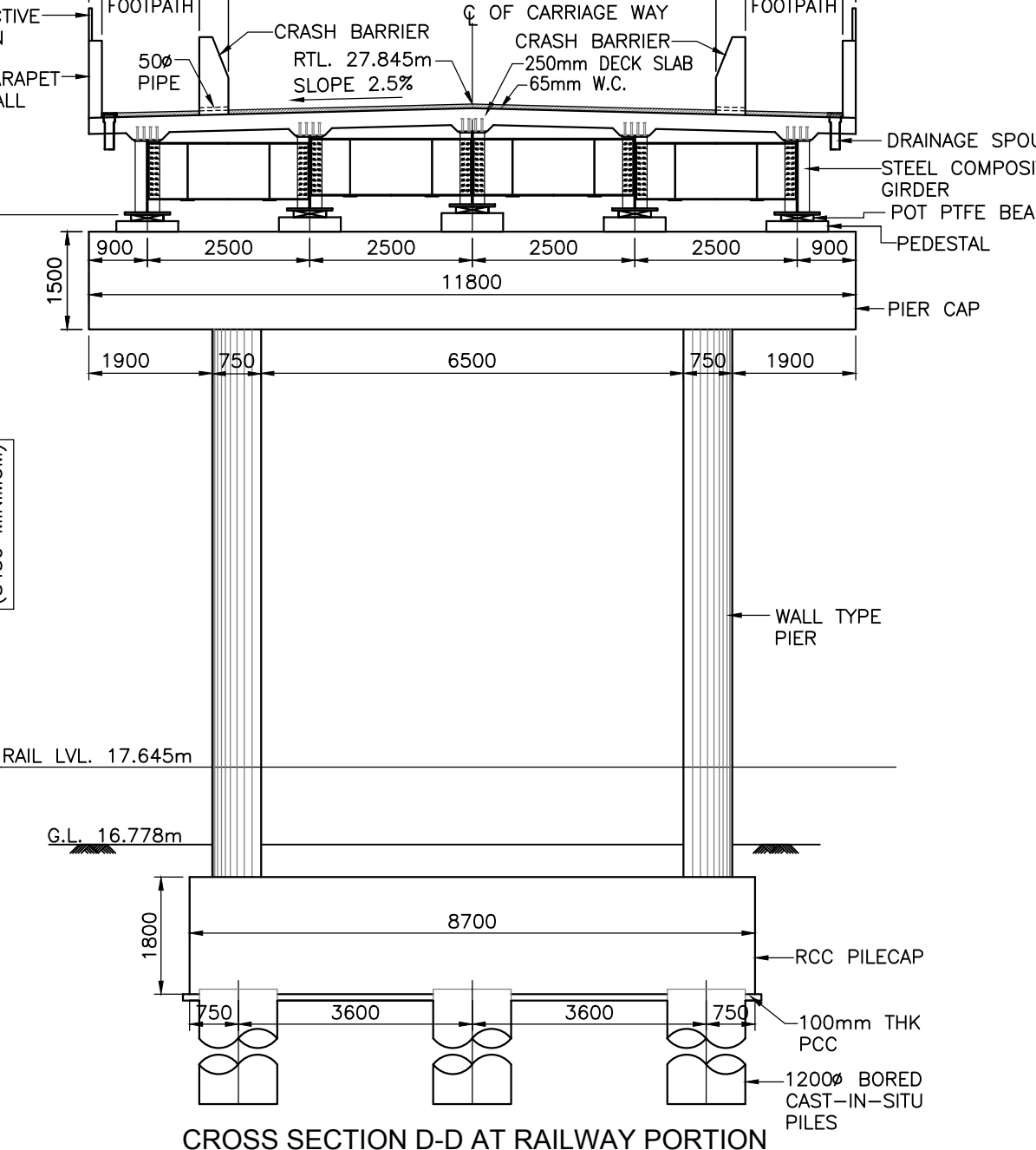
DETAILS OF PIER P17
(SCALE: 1:100)



DETAILS OF PIER P19
(SCALE: 1:100)



CROSS SECTION C-C AT RAILWAY PORTION
(SCALE: 1:100)



CROSS SECTION D-D AT RAILWAY PORTION
(SCALE: 1:100)

- NOTES:
- ALL DIMENSIONS ARE IN MILLIMETER AND LEVELS ARE IN METRES UNLESS WRITTEN OTHERWISE.
 - NO DIMENSION SHALL BE SCALED FROM THIS DRAWING ONLY WRITTEN DIMENSION SHALL BE FOLLOWED.
 - DIMENSION IN RECTANGLE ARE OBLIGATORY.
 - SHAPE AND SIZE OF STEEL COMPOSITE GIRDERS POT. PTFE BEARINGS, PEDESTALS, R.C.C COLUMN, PIER CAP AND FOOTING SHOWN IN THIS DRAWING ARE TENTATIVE AND ARE SUBJECT TO CHANGE IN FINAL EXECUTION DRAWING AND DESIGN.
 - DRAWING AND DESIGN OF APPROACHES SHALL BE DECIDED BY ROAD AUTHORITY.
 - PROPOSED WORK SHOWN IN RED, FUTURE WORK IS IN RED DOTTED AND EXISTING WORK IS SHOWN IN BLACK.
 - A MINIMUM VERTICAL CLEARANCE OF 8430mm SHALL BE PROVIDED FROM RAIL TOP LEVEL (HIGHEST) TO BOTTOM OF PROPOSED STEEL PLATE GIRDER AS PER RAILWAY BOARD'S LETTER NO. 80/ND/50/10 DATED 15/10/1986.
 - ANTI-CRASH-BARRIER SHALL BE PROVIDED AS PER H.O.S.T. STANDARD.
 - NET SAFE BEARING CAPACITY FOR PILE FOUNDATION HAS BEEN CONSIDERED AS 350 T/M².
 - DEPTH OF FOUNDATION MAY BE ALTERED BY ENGINEER-IN-CHARGE TO SUIT THE SOIL STARTA MET WITH AT SITE.
 - ACTUAL LOCATION OF R.O.B SHALL BE DECIDED BY ENGINEER IN CHARGE IN CONSULTATION WITH ROAD AUTHORITIES AT THE TIME OF ITS CONSTRUCTION.
 - CONCRETED CEMENT CONCRETE MIX SHALL BE AS PER RAILWAYS SPECIFICATION ISSUED AS ON DECEMBER-2000 OR LATEST.
 - DESIGN & DRAWING SHALL BE PROOF CHECKED FROM REPUTED CONSULTANTS BEFORE EXECUTION.
 - WHILE INVITING TENDERS THE SPECIFICATION ENCLOSED SHALL BE A PART OF CONTRACT FOR CONSTRUCTION OF BRIDGE OVER THE RAILWAY SPAN.
 - NO PLASTERING AND CEMENT WORK PROTECTIVE COATING OVER THE CONCRETE IS PERMITTED. NO REPAIR SHALL BE PERMITTED. ANY BAD WORK WILL SUMMARILY QUALIFY FOR REJECTION OF MEMBER /ELEMENT.
 - A SEPRATE LAUNCHING SCHEME HAS TO BE SUBMITTED DULY APPROVED BY CE/C/NORTH BEFORE COMMENCEMENT OF WORK.
 - POT-PTFE BEARING SHALL BE PROVIDED AT ALL THE LOCATION.
 - GUARD RAIL WILL BE PROVIDED AS PER STANDARD DRAWING.
 - REINFORCEMENT BAR SHALL BE PROVIDED WITH PROTECTIVE COATING AS PER PARA 7.1.5 OF CONCRETE BRIDGE CODE CORRECTION SLIP NO.1 DATED 26.4.2000 AND LATEST.
 - FOLLOWING GRADES OF CONCRETE SHALL BE USED.
DECK SLAB -M40
PEDESTALS/PIERCAP/PIER -M35
FOUNDATION -M35
LEVELLING COURSE -M20
ANTI-CRASH BARRIER -M35
APPROACH SLAB -M30
 - GRADE OF STEEL SHALL BE Fe-500 CONFIRMING TO IS:1786-2008 & UPDATED WITH LATEST CORRECTION SLIP.
 - ONE LANE OF IRC 70R OR 2 LANES OF CLASS A WHICH EVER PRODUCE MAXIMUM EFFECT SHALL BE CONSIDERED IN DESIGN.
 - STANDARD PROTECTIVE SCREEN ABOVE EACH TRACK ON EITHER SIDE OF ROB ON PARAPET/ CRASH BARRIER SHALL BE PROVIDED AS DIRECTED BY ENGINEER-IN-CHARGE.
 - RAILWAY ORGANIZATION WILL CONSTRUCT P17 TO P20. A1 TO P16 AND P21 TO A2 & RE WALL WILL BE EXECUTED BY PWD VALSAD.
 - STANDARD 36.0M RDSO DRAWING NO. RDSO/B/-11756/R TO RDSO/B-11758/10R & RDSO/B-11759/R SHALL BE USED FOR SUPERSTRUCTURE.
 - SEISMIC ZONE IS III.
 - SKEW ANGLE IS 7.7°.

Rev.	Revision Details	Date	PURPOSE
R1	REVISED AS PER RAILWAY COMMENT ABOVE DFCC TRACK 36.0m TO 2x18.0m RDSO SPAN REVISED.	29.08.2019	FOR APPROVAL
RO	FIRST SUBMISSION	22.08.2019	FOR APPROVAL

Drawn	PRASHANT
Designed	AAKASH
Checked	L.K.MISHRA

Owner: WESTERN RAILWAY

Client: DEDICATED FREIGHT CORRIDOR CORPORATION OF INDIA LIMITED, MUMBAI

SOWIL Limited consulting engineers
GOODWILL AVENUE, OFFICE NO. 02, A WING, FIRST FLOOR,
PLOT NO. 01, SECTOR-40, NERUL (WEST), NAVI MUMBAI - 400 706

Name of Work: CONSTRUCTION OF PROPOSED ROAD OVER BRIDGE IN LIEU OF LC NO. 96 BETWEEN ATUL & VALSAD RAILWAY STATION AT CH. 196/26-28.

Drawing No: SOWIL / WR / GAD / ATUL-BL / 02. Rev. R1

Scale: AS SHOWN Sheet: 2 OF 2

Title: GENERAL ARRANGEMENT DRAWING (RAILWAY PORTION)

This document is issued for the party which commissioned it and for specific purposes connected with the captioned project only. It should not be relied upon by any other party or used for any other purpose. We accept no responsibility for the consequences of this document being relied upon by any other party, or being used for any other purpose, or containing any error or omission which is due to an error or omission in data supplied to us by other parties. This drawing is the property of sowil Limited. It should not be used, copied or reproduced for other purpose.

FOR PWD (VALSAD)	DFCCIL, MUMBAI	MUMBAI DIVISION W. RAILWAY	MUMBAI DIVISION WR (OPEN LINE H.Q.)	CONSTRUCTION DIVISION WESTERN RAILWAY	HEAD QUARTERS
------------------	----------------	----------------------------	-------------------------------------	---------------------------------------	---------------

CPM	DY. CPM (ENGG.)	ADEN	SR. DEN (NE) CCG	SR. DOM	XEN(ROB)	DY.CE(BR-I)	CHIEF BRIDGE ENGG.	SSE (WKS)	XEN(C)	DY. CE (C)	DY. CSTE (C)	DY. CEE (C)	SSE (DRG)	AXEN(C) D	DY. CE (C) D	C.E.(C) SOUTH
-----	-----------------	------	------------------	---------	----------	-------------	--------------------	-----------	--------	------------	--------------	-------------	-----------	-----------	--------------	---------------